

**Archaeological Impact Assessment of the  
Abbey Quarter Temporary Coach & Car Park  
(Urban Blocks 12 and 13), Gardens Td.,  
Kilkenny City**



**Prepared for Kilkenny County Council  
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## **Kilkenny County Council Archaeological Impact Assessment of the Abbey Quarter Temporary Coach & Car Park (Urban Blocks 12 and 13), Gardens Td., Kilkenny City**

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## Executive Summary

Kilkenny County Council propose to develop a temporary car and coach park in part of the Abbey Quarter Masterplan Area between the River Breagagh and the Central Access Scheme / St Francis Bridge (Master Plan Urban Blocks 12 and 13 and part of Public Realm C). The proposed development will provide for seven coach parking spaces and one hundred and thirty-two car spaces and will be accessed from St Francis Bridge. The proposed development will be for a fixed duration of up to five years, pending the future development of this area of the Abbey Quarter Site in accordance with the provisions of the Abbey Quarter Masterplan.

This document contains an archaeological impact assessment of this proposed development, describing the archaeological and historical background to the area, a summary of the results of archaeological test excavations in this area directed by Jon Stirland ACSU, an archaeological impact statement for the proposed car park development, and a set of measures to mitigate the archaeological impact of the proposed development.

This assessment has found that the archaeological impacts of the proposed development are not significant.

The development has been designed to protect the upstanding archaeological remains on site, the Bull Inn and Evans Turret, behind fencing.

Subsurface archaeology is at least 0.5m below ground. The majority of the works will involve building up the level of the ground to create a level surface for the car park, and new service trenches are avoided by renovating existing services or laying cables for lighting on the surface.

In the one area that will be reduced, the ground reduction will only consist of the removal of the modern surface layer of concrete c.0.3m in depth.

Archaeological mitigation to offset any adverse impacts comprises monitoring by a licence eligible archaeologist of the placement of all protective fencing, ground reduction works and the excavation of bases for new lighting poles.

In the unlikely event that archaeology is impacted or previously unidentified archaeologically significant features are encountered during construction works for the proposed car and coach parks these works will be stopped, Kilkenny County Council and the National Monuments Service will be informed, the archaeology will be fenced off and protected, and a suitable measure will be agreed to mitigate the archaeological impacts.

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## Terms and Definitions

TERM	DEFINITION
<b>AMS</b>	Archaeological Management Solutions Ltd
<b>BH</b>	Bore Hole
<b>CAS</b>	Central Access Scheme
<b>DAHRRGA</b>	Department of Arts, Heritage, Regional, Rural and Gaeltacht Affairs
<b>DoEHLG</b>	Department of Environment, Heritage and Local Government (precursor to DAHRRGA)
<b>IAI</b>	Institute of Archaeologists of Ireland
<b>ITM</b>	Irish Transverse Mercator
<b>KCC</b>	Kilkenny County Council
<b>KKAP</b>	Kilkenny Archaeological Project (Ó Drisceoil et al. 2008)
<b>NIAH</b>	National Inventory of Architectural Heritage
<b>NMI</b>	National Museum of Ireland
<b>NMS</b>	National Monuments Service
<b>RMP</b>	Record of Monuments and Places
<b>SMR</b>	Sites and Monuments Record
<b>TP</b>	Test Pit
<b>WS</b>	Window Sample
<b>UAS</b>	Urban Archaeological Survey

## Key Stakeholders

- Kilkenny County Council
- National Monuments Service
- National Museum of Ireland

## Introduction

Kilkenny County Council (KCC) has acquired the former Smithwick's Brewery site in Kilkenny from Diageo. This site will be incorporated into the Abbey Creative Quarter Masterplan (Reddy Architecture and Urbanism 2013; Kilkenny County Council 2015), which is a planned redevelopment of 8.25ha in the centre of Kilkenny City (<http://ourplan.kilkenny.ie/brewery-site>; Figure 1). The Abbey Creative Quarter Masterplan area has been divided into fourteen Urban Blocks (1–14). For the purpose of archaeological assessment the Public Realm works have been divided into four areas (A–D) (Figure 2).

AMS have been retained by Kilkenny County Council as Project Archaeologists, to prepare archaeological strategies for the Abbey Creative Quarter Masterplan areas, and to oversee archaeological test excavations as part of these strategies.

Kilkenny County Council propose to use part of the area between the Breaghagh River and the Central Access Scheme (Urban Blocks 12 and 13 and part of Public Realm C) as a temporary car and coach park for a period of up to five years, after which time the area will be developed as set out in the Masterplan.

This report is an archaeological impact assessment of this proposed temporary car park development. It contains an archaeological and historical background and descriptions of the findings of previous excavations, including the most recent test excavations for the Abbey Creative Quarter Archaeological Strategy from early 2018, and a description of the proposed development. The impacts of the proposed development are set out in an archaeological impact statement. Finally, measures are set out detailing how these impacts can be avoided or mitigated.

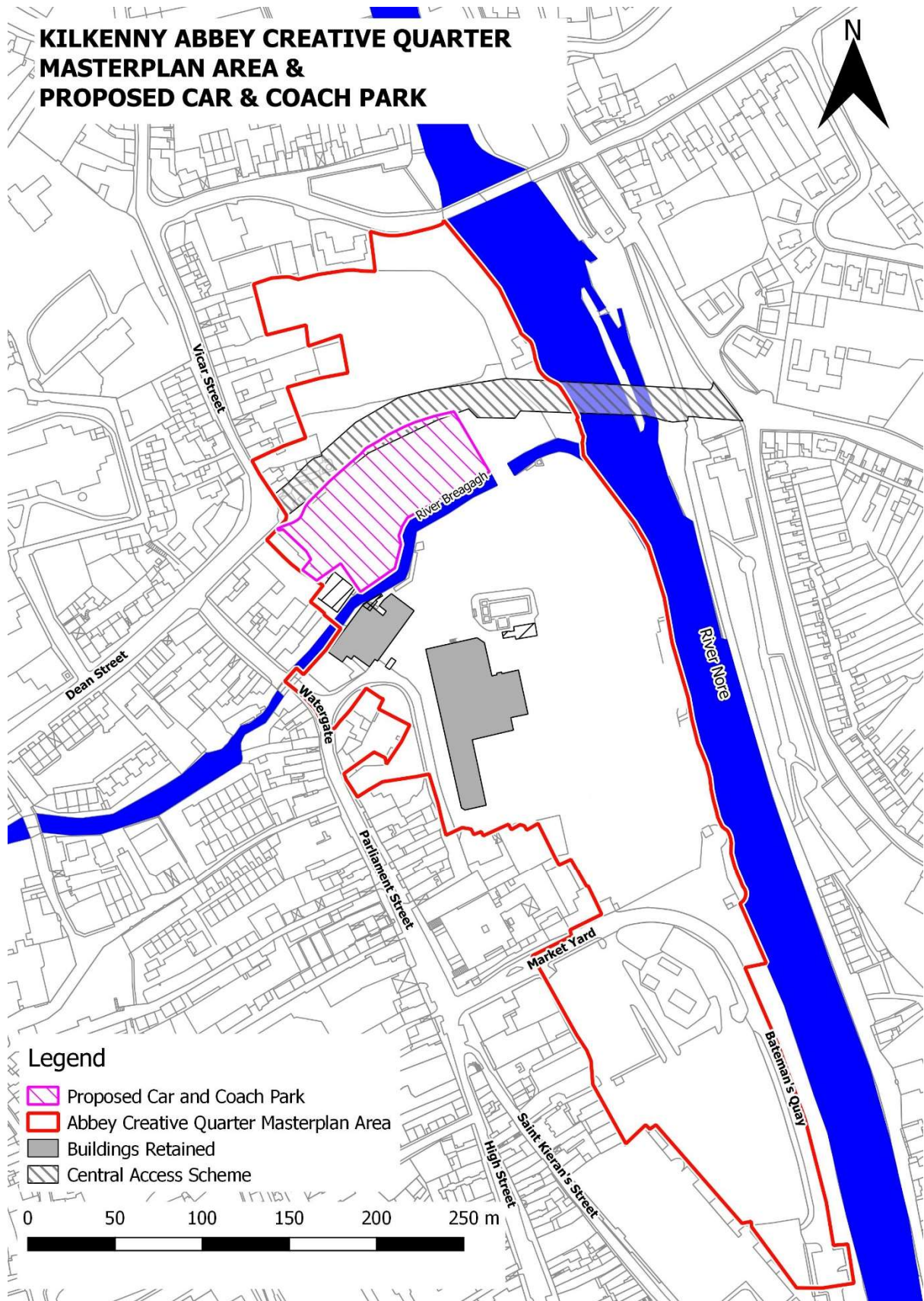


Figure 1: Masterplan area, Kilkenny City, with the location of the proposed car and coach park



## Site Location

The proposed development, which is the subject of this report (Urban Blocks 12 and 13 and partly in Public Realm C), consists of an area measuring 0.57ha located to the north of the Breagagh river in the townland called Gardens.<sup>1</sup>

The development area is bounded to the south and east by the Breagagh river behind a modern river wall; to the north by the Central Access Scheme and St Francis' Bridge; to the west by an open yard area behind properties on Saint Canice's Place; and to the east by an open concrete-covered yard, and the River Nore.

The development area had formerly been part of the Diageo brewery site accessed from the rest of the brewery site via a concrete bridge across the Breagagh (Public Realm C) constructed c. 1974 (Courtney Deery 2014, 97) (Figure 1 and 2; Plate 1).



Plate 1: Proposed temporary car and coach park development area looking west

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<sup>1</sup> Centre coordinates ITM 650432, 656402.

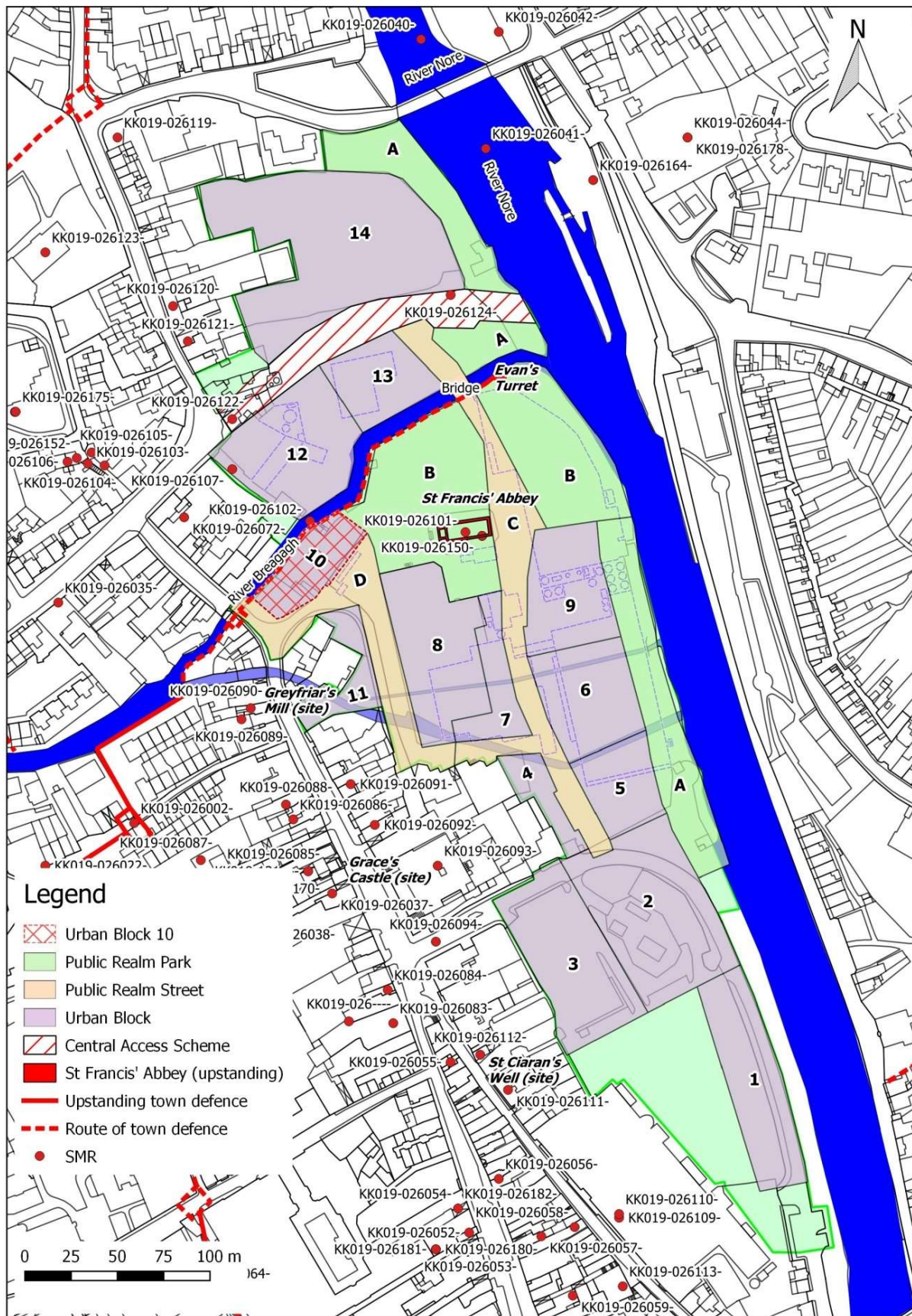


Figure 2: Abbey Creative Quarter Masterplan areas

## Archaeological and Historical Significance

### Recorded Archaeology

The proposed development area lies within the boundaries of the archaeologically significant historic town of Kilkenny, which is a designated RMP site (KK019-026).

It is located on the opposite side of the River Breagagh to the precinct of St Francis' Abbey, a Franciscan friary (KK019-026101-) and a National Monument (No. 72), and Kilkenny City's walls (Oxford Archaeology 2005), which are also designated a National Monument under government policy (DoEHLG 2008).

Courtney Deery (2014) noted that Gardens townland, immediately north of the River Breagagh, is shown on historical maps as comprising burgage plots and large open areas, probably meadow land used for summer grazing. The river is shown extending into this area on earlier maps (Figures 3, 4 and 5).

The west of Urban Block 12 contains the upstanding remains of the 'Bull Inn' (SMR KK019-026107-; Figure 2 and Figure 3), an early seventeenth century hostelry fronting onto Saint Canice's Place (formerly Bull Alley Lane). The 'Bull Inn' was a three-storey gable-fronted stone building, the apex of which was surmounted by a stone chimney, according to an illustration by Reverend James Graves from 1841 (Prim 1862, 176–77; Figure 8). The 'Bull Inn' was largely demolished in the nineteenth century, and what remains consists of the east wall (height c. 4.5m; length c. 15m), which survives up to first-floor level, and some of the west wall (height c. 2.5m; length c. 15m), which also forms part of the west boundary of Urban Block 12, with a small portion of the rear return (thickness 0.8–0.9m) on both the east and west walls (Courtney Deery 2014, 30–31, 99–101) (Plate 2).

The remains of the springing of a bridge (SMR KK019-026102-) are visible in the walls flanking the River Breagagh, which formerly led from Public Realm B across the River Breagagh to a lane outside the west boundary of Urban Block 12 (formerly known as Bull Alley Lane) leading north to Saint Canice's Place (Figure 2). This may be the remains of 'Cotterals Bridge' (Bradley 2000, 21; Courtney Deery 2014, 29–30; Figure 4–Figure 7).

Historical maps show two ranges of buildings at the west end of Urban Block 12. However, most of the area to the south and east of Urban Block 12 appears to have been an enclosed garden and orchard area (Figure 3–Figure 6). Historical maps show no buildings in Urban Block 13 in the area to be developed as the temporary car and coach park, although the area is crossed by a number of boundaries (Figure 3–Figure 7), including what appears to be watercourses running through the area into the River Breagagh (Figure 3–Figure 5). The north side of Urban Block 13 also appears to overlay part of Mill Lane, which ran from a mill on the River Nore west to Vicar Street (Figure 4–Figure 7).

Cartographic evidence suggests the area of Urban Block 13, similar to Urban Block 12, appears to have been an enclosed garden and orchard area.

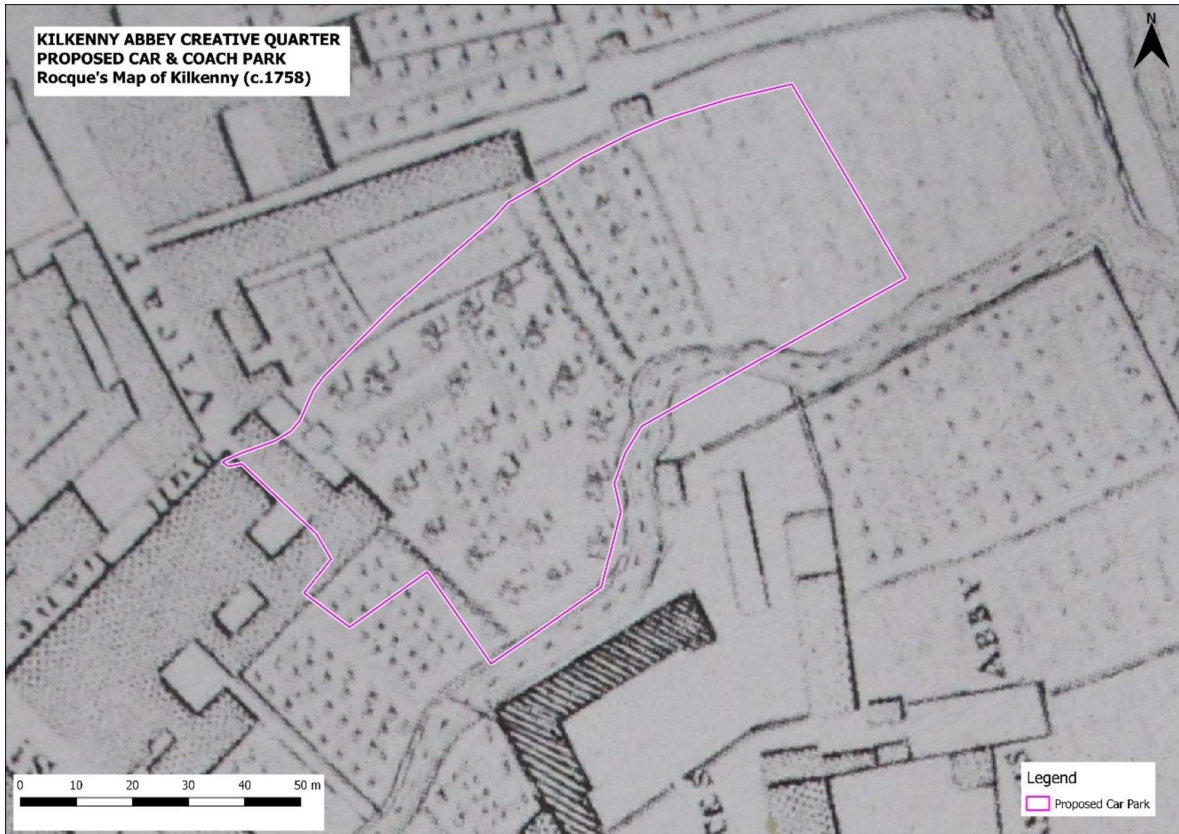


Figure 3: Proposed development shown on Rocque's map (1758)

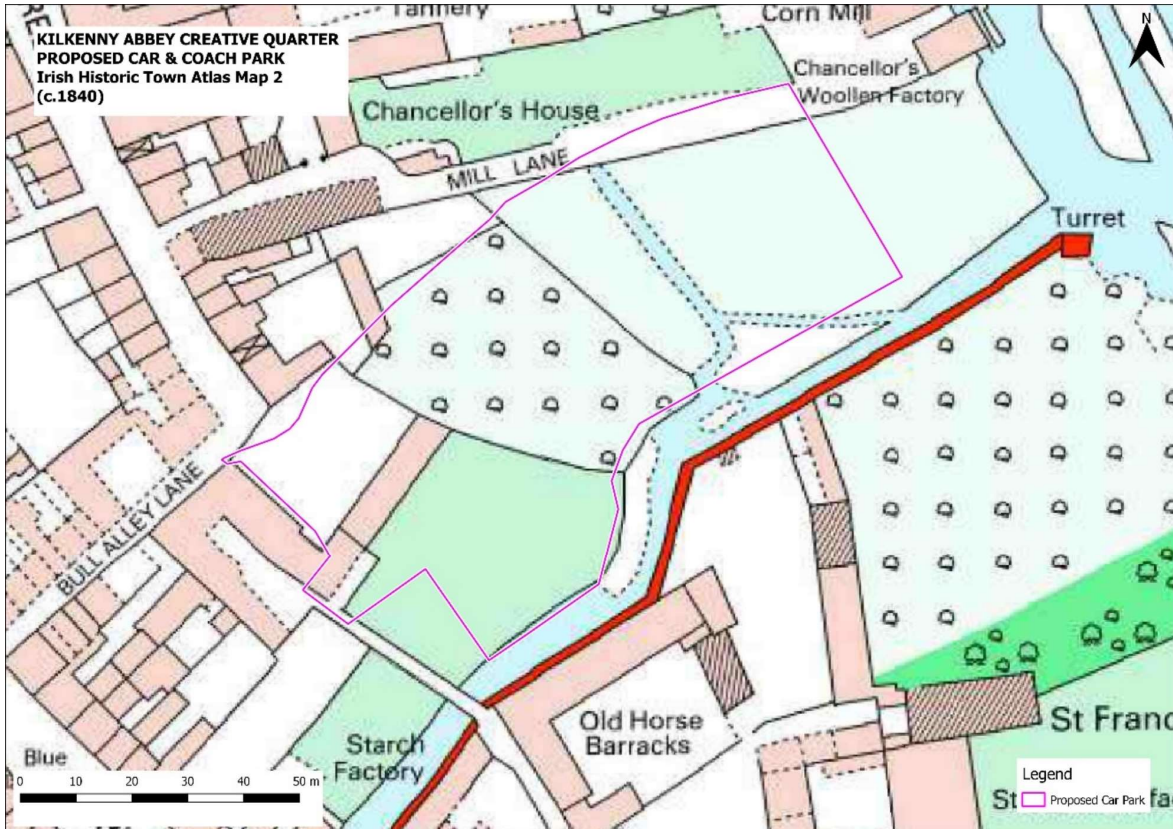


Figure 4: Proposed development shown on the Irish Historic Towns Atlas, Map 2 (c. 1840).

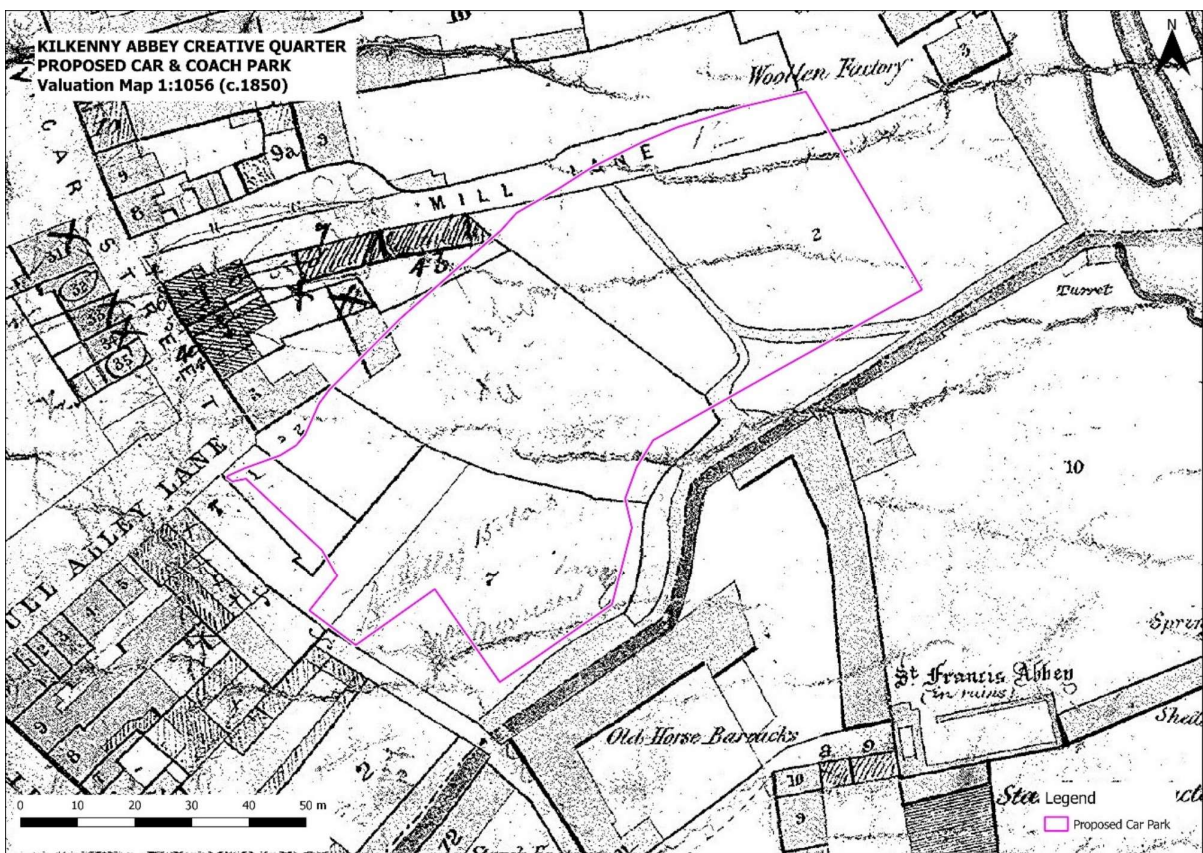


Figure 5: Proposed development shown on Town Plan map (1850)

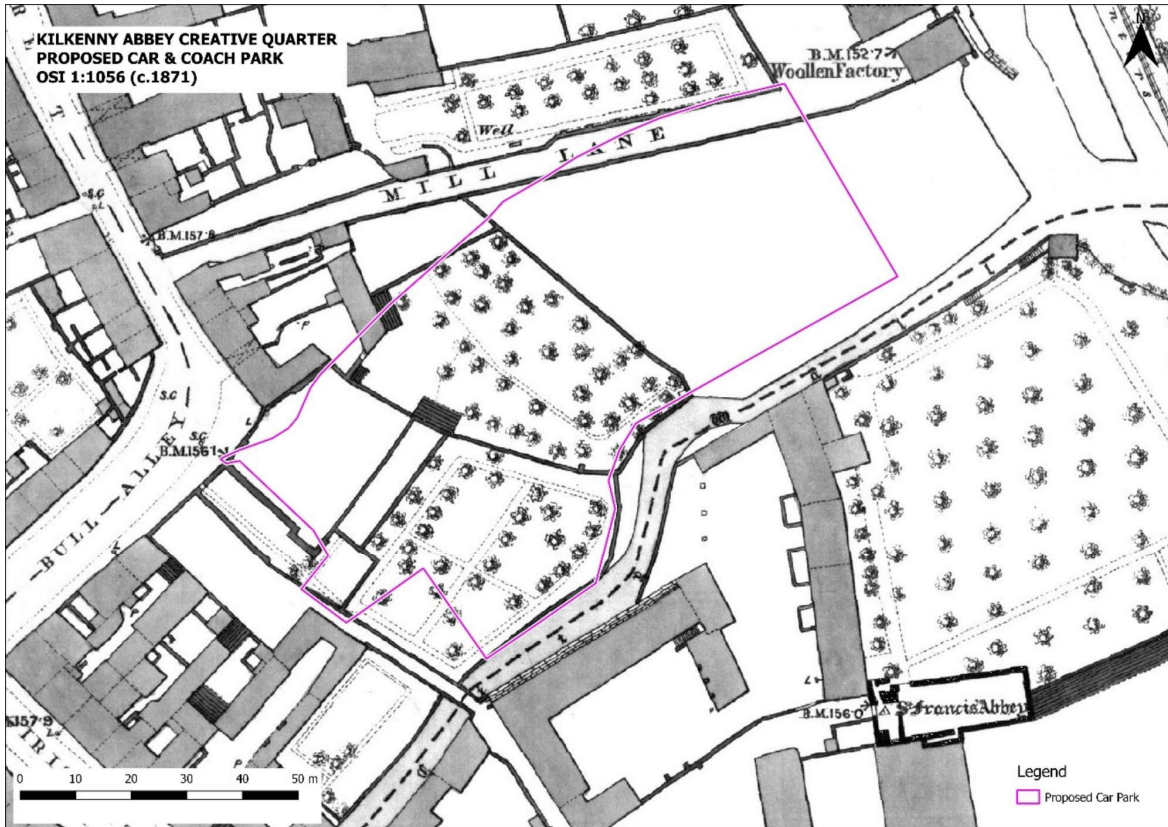


Figure 6: Proposed development shown on Town Plan map (1871)

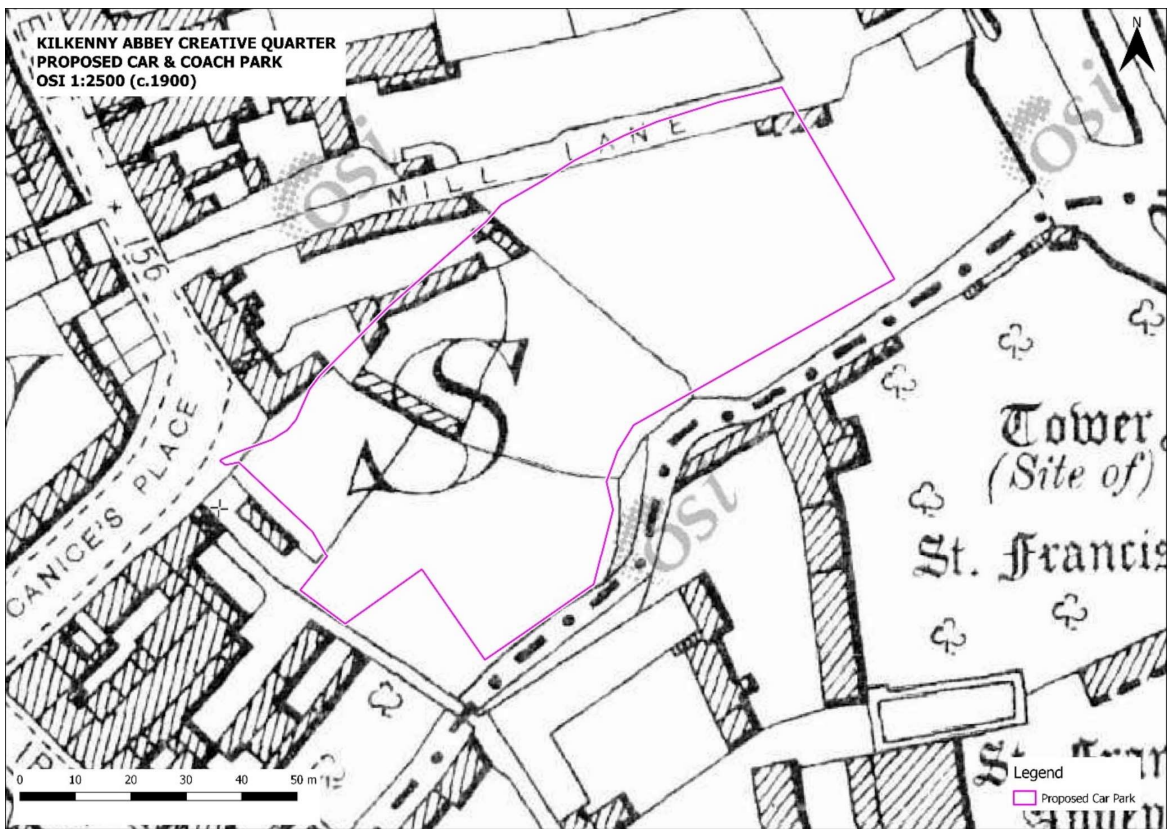


Figure 7: Proposed development shown on OSI 25 Inch map (1900)



**Figure 8: Depiction of the Bull Inn from Plate 2: Remains of the Bull Inn 1841 (Prim 1862, 177).**



**Plate 3: North end of the Bull Inn and the preserved gable of No. 22 Vicar Street**

## Archaeological Excavations before 2018

Prior to test excavations in 2018, two archaeological excavations were carried out in Urban Block 12 and 13, with one additional excavation to the immediate north of the proposed development (Ó Drisceoil et al. 2008; Figure 9).

The first excavation in the proposed development area consisted of monitoring the excavation of service trenches in the hops store yard area in 2007 (O'Meara 2007; KKAP-203; 04E0694ext). This consisted of monitoring the excavation of a trench c. 70m long, 1m wide and 2m deep, with an oil interceptor tank trench in Urban Block 12. The predevelopment ground level was 45m to 44.80m OD west to east. The stratigraphy identified was as follows:

- 45–44.7m: Two layers of concrete yard surface.
- 44.7–44.5m: Mid grey brown sandy gravel: sterile imported hardcore.
- 44.5–44.3m: Grey gravelly sand and mixed red brick and mortar building rubble.
- 44.35–43.95m: Grey silt sand: made ground with inclusions of masonry and lime mortar building rubble and slate. This layer became gradually shallower from east to west.
- 44.05–42.55m: Dark grey brown sandy silt alluvial deposits: river bank.
- 42.55–42.3m+: River gravel.

In Urban Block 13 a trench 60m long, with intersecting trenches totalling 20m long, 1m wide and 2m deep was archaeologically monitored under the same licence (04E0694ext). The ground level ranged from 44.7m to 44.4m OD west to east. The stratigraphic sequence consisted of the following:

- 44.7–44.6m: Concrete yard surface.
- 44.6–44.05m: Mid grey brown sandy gravel. Sterile imported hardcore.
- 44.05–43.9m: Second concrete yard surface.
- 43.9–43.4m: Grey gravel sand and mixed red brick and mortar building rubble.
- 43.4–42.7m+: Dark grey black silt with occasional inclusions of red brick.

No archaeological material was found during these works. The excavation identified a second modern concrete yard surface c. 0.65m below the modern ground surface. This overlay a c. 0.50m layer of made ground, which in turn overlay a silty layer with brick inclusions.

The second excavation was carried out by Colm Flynn in 2015. Flynn directed excavations in advance of the Central Access Scheme in the property 20–22 Vicar Street to the northwest of Urban Block 12 (Flynn 2015; E4437). These excavations consisted of test trenching and building surveys at 20–22, including one test trench (no. 3) that was located immediately north of Urban Block 12.

Test Trench 3, located parallel to the gable wall of 22 Vicar Street, was excavated to a maximum depth of 1.2m. This trench uncovered the remains of a box drain; a cobble surface; organic layers; and several



walls, some of which extended into Urban Block 12 and some of which abutted the gable wall of 22 Vicar Street to the north; a hearth next to this gable wall; a line of post holes; and a floor surface. These remains all appear to be part of a medieval to post-medieval structure extending into Urban Block 12.

Flynn also carried out test excavations at 21–22 Vicar Street. Excavations were carried out to a maximum depth of 1.2m beneath the modern surface and uncovered floor surfaces for modern houses; garden soils; the foundations of modern garden sheds; walls and hearths associated with post-medieval buildings; post-holes; a well; box drains; modern ceramic drains; cobble surfaces; and five kilns.

Subsequent excavations revealed the foundations of five buildings within and to the rear of 20–22 Vicar Street. One building had the remains of wood plank flooring. Flynn dated these buildings to the eighteenth / nineteenth century. In addition to the surviving timbers, deposits with organic remains including animal bone, cattle horn and shell, were discovered. Over one thousand sherds of medieval and post-medieval pottery, one hundred and fifty clay pipe fragments, metal finds, as well as glass, and eighty architectural stone artefacts were recovered during the demolition works and subsequent excavations. Archaeological levels were identified at 42–43m OD.

The third excavation, also carried out by Colm Flynn in 2013, was located to the immediate north of Urban Blocks 12 and 13 (Flynn 2013). These test excavations were carried out in advance of the Central Access Scheme where the new road crossed the former Diageo brewery site. Flynn excavated six test trenches. Two (Test Trenches I and II) were located adjacent to Urban Block 12. Test Trench I was 15m long, 2.4m wide and a maximum 2m deep. The stratigraphy of this trench is summarised as follows:

- 0–0.15m: Concrete
- 0.15–0.35m: Hardcore
- 0.35–0.95m: Building rubble and garden soil
- 1m: Archaeology in the form of structural walls, a floor and a box drain
- 1–2m: Natural dark grey bluish silt alluvial deposits.

Test Trench II was 20m long, 2.4m wide and a maximum 2m deep. The trench stratigraphy is summarised as follows:

- 0–0.15m: Concrete
- 0.15–0.35m: Hardcore
- 0.35–0.95m: Building rubble and garden soil with finds of modern glass, ceramics and metal.
- 1m: Archaeology in the form of a brick wall, a flag floor surface, hearth, chimney.

- 1–2m: Natural dark grey bluish silt alluvial deposits.

Flynn excavated three test trenches (Test Trenches III to V) in the CAS adjacent to Urban Block 13.

Test Trench III was 15m long, 2.4m wide and a maximum 2m deep. The trench stratigraphy is summarised as follows:

- 0–0.15m: Concrete
- 0.15–0.35m: Hardcore
- 0.35–0.95m: Building rubble and garden soil with finds of modern glass, ceramics and metal.
- 1m: Archaeology in the form of a floor surface with in-situ timber
- 1–2m: Natural dark grey bluish silt alluvial deposits.

Test Trench IV was 15m long, 2.4m wide and a maximum 2m deep. The trench stratigraphy is summarised as follows:

- 0–0.15m: Concrete
- 0.15–0.35m: Hardcore
- 0.35–0.95m: Building rubble and garden soil with finds of modern glass, ceramics and metal.
- 1m: Cobbled surface, possibly associated with Mill Lane, and a possible compacted floor surface.
- 1–2m: Natural dark grey bluish silt alluvial deposits.

Test Trench V was 15m long, 2.4m wide and a maximum 2m deep. The trench stratigraphy is summarised as follows:

- 0–0.15m: Concrete
- 0.15–0.35m: Hardcore
- 0.35–0.95m: Building rubble and garden soil with finds of modern glass, ceramics and metal.
- 1–1.4m: Natural dark grey silt alluvial deposits.

These test excavations indicated a general depth to natural of c. 1m to the north of Urban Block 13.

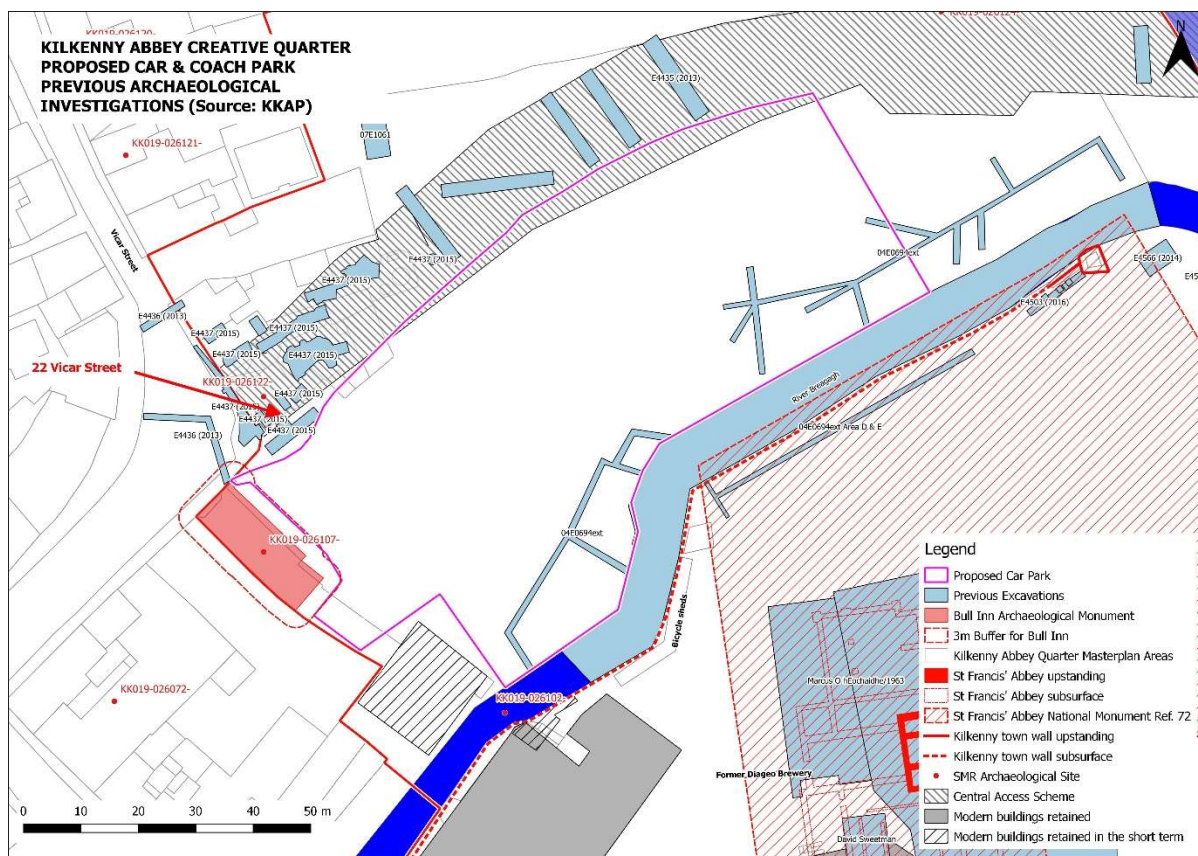


Figure 9: Previous archaeological excavations

## Geotechnical Investigations

Courtney Deery archaeologically monitored eight geotechnical investigations in Urban Block 12 (2014, appendix D) (Figure 14). These investigations found that Urban Block 12 is generally covered with a concrete surface of 0.15–0.30m, reinforced concrete or tarmac, over c. 1.1–3.2m (BH A) of made ground with mortar and brick inclusions, which in turn overlay organic deposits and river gravels and silts. These investigations can be summarised as:

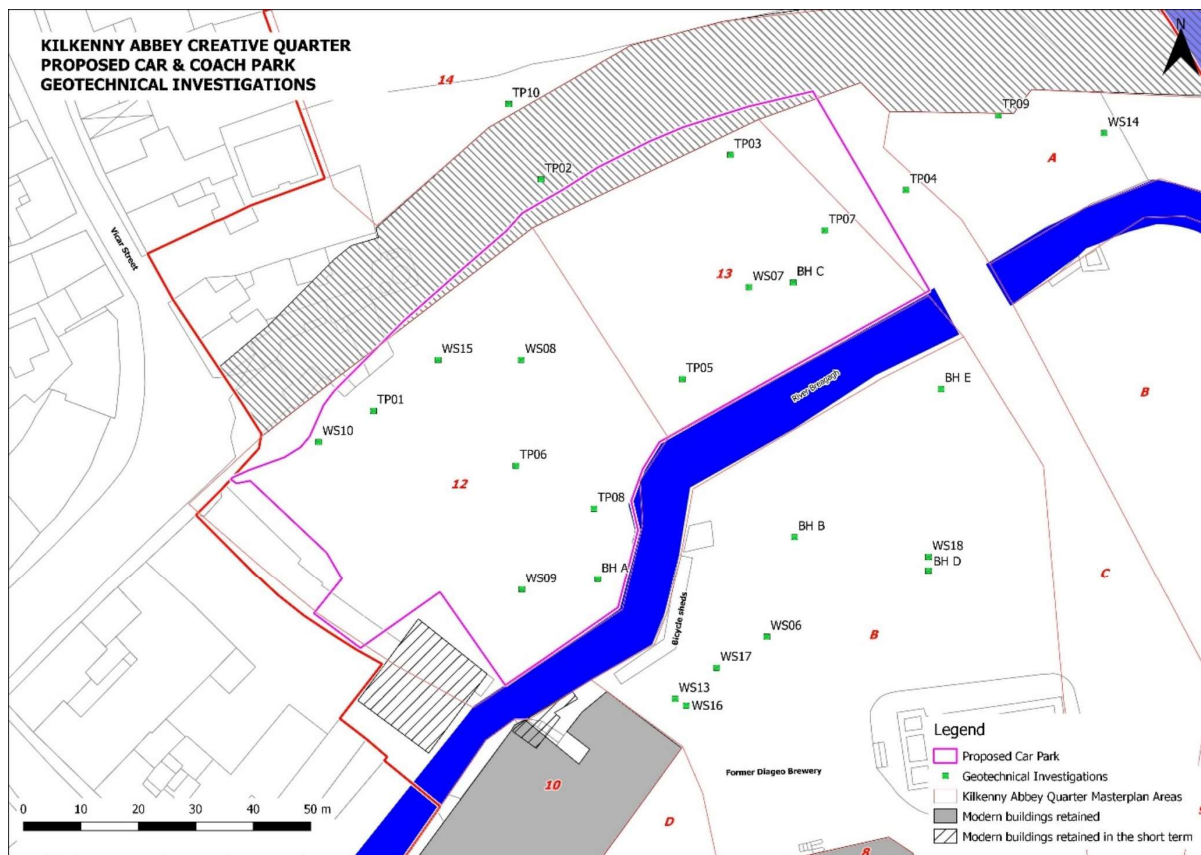
Table 1 Summary of geotechnical investigations in Urban Block 12

Pit	Depth of Archaeology		
	(m)	ITM_East	ITM_North
BH A	3.4	650434	656367
TP01	2.6	650395	656396
TP06	2.25	650420	656387
TP08	2.2	650433	656379
WS08	2.1	650421	656405
WS09	2.2	650421	656365
WS10	2.2	650386	656391
WS15	1.2	650406	656405

Geotechnical investigations found that Urban Block 13 is generally covered with a concrete surface of 0.15–0.30m of concrete, reinforced concrete or tarmac, over c. 1.1–3.2m (BHA) of made ground with mortar and brick inclusions, which in turn overlay organic deposits and river gravels and silts. A second concrete layer was encountered in TP07 at 0.65m depth below the surface. These investigations can be summarised as:

**Table 2 Summary of geotechnical investigations in Urban Block 13.**

Pit	Depth of Archaeology (m)	ITM_East	ITM_North
BH C	1.95	650468	656419
TP03	1.9	650457	656441
TP05	2.3	650449	656402
TP07	0.65	650474	656428
WS07	2.2	650460	656418



**Figure 10: Geotechnical investigations**

### Summary of the Results of Archaeological Test Excavations in 2018: 17E0642

Specific archaeological strategies for targeted test trench excavations are detailed for each of the Urban Block and Public Realm areas of the Abbey Creative Quarter Masterplan area by AMS (2016a–m). AMS have been appointed by Kilkenny County Council as Project Archaeologists, to oversee the implementation of these strategies. ACSU Ltd were subsequently appointed to carry out test excavations in Urban Blocks 5, 6, 7, 9, 11, 12 and 13, and Public Realms B and C. A total of seventeen test trenches were scheduled to be excavated under Section 14 Ministerial Consent (C853 / E4950), with a further twenty-seven test trenches within the Abbey Creative Quarter Masterplan area, but falling outside the National Monuments' boundaries, to be excavated under Section 26 licence (17E0642). Excavations were carried out in February and March 2018, directed by Jon Stirland ACSU, during which forty-one archaeological test trenches were excavated.<sup>2</sup> Eleven of these test trenches were excavated in Urban Blocks 12 and 13 under licence (17E0642), within the footprint of the proposed car and coach park. The reports for this testing are currently being prepared.

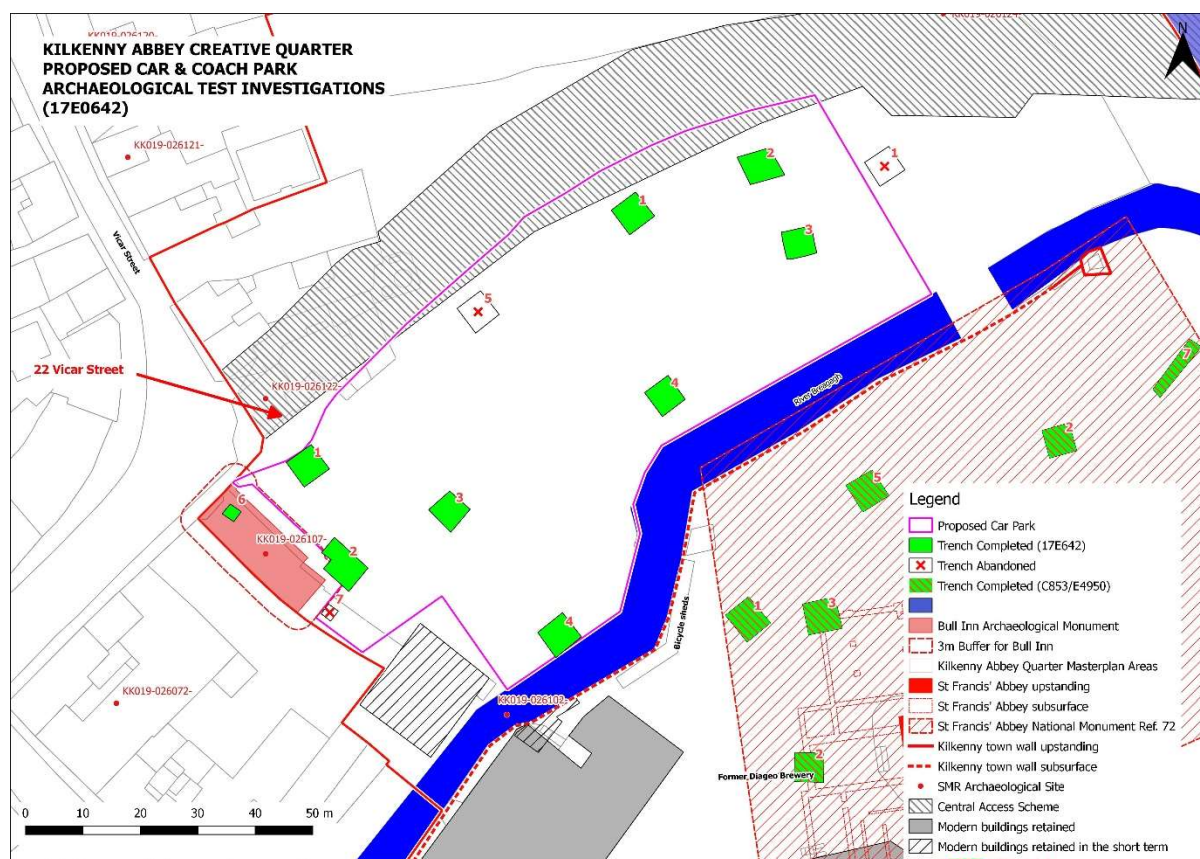


Figure 11: Test excavation 17E0642

<sup>2</sup> Reports pending a second round of test excavations shortly to be carried out by ACSU in Urban Blocks 10 and 11 and in Public Realm B in the Abbey Creative Quarter Masterplan Area.

Each test trench had a specific aim, generally targeted on specific features and areas, as identified in the archaeological strategies (AMS 2016j–k). Testing identified structural remains for buildings dating to at least the eighteenth century (and likely the early seventeenth century) in Test Trenches 1 and 2 in Urban Block 12 (Plate 4 and Plate 5). However, the archaeology here is located at least 0.5m below the current ground surface. Elsewhere in Areas 12 and 13 archaeology was found between 1m and 2.78m below the current ground surface, below a mixture of modern made ground, second concrete surfaces and modern buildings. The archaeology here appears to be a mixture of waterlogged reclamation deposits and riverine deposits over natural gravels. The results of each test trench are summarised in Table 3 below.



**Plate 4: Masonry walls in Test Trench 1, Urban Block 12, with the preserved gable of No. 22 Vicar Street in the background.**



Plate 5: Masonry walls and cobble surfaces in Test Trench 2, Urban Block 12

Table 3: Summary results from Test Excavations: Urban Blocks 12 and 13

Area	Trench No.	Trench Aims	Summary of Results	OD Top of Trench m	OD Top of Archaeology m	OD Base of Trench	Depth to Archaeology m	Depth of Trench m	Area of Trench m <sup>2</sup>	Centre ITM_E	Centre ITM_N
Urban Block 12	1	Investigate medieval/post-medieval buildings	Cobbled surfaces for building interiors; masonry walls (late 16th-18th c); pits into natural subsoils.	44.77	44.27	42.27	0.5	2.5	28.42	650383	656387
Urban Block 12	2	Investigate medieval/post-medieval buildings	Cobbled surfaces for building interiors; masonry walls (late 16th-late 19th c).	44.773	44.073	44.496	0.7	0.277	38	650390	656370
Urban Block 12	3	To investigate possible medieval back gardens and post-medieval structures	Reclamation deposits (18-20th c); masonry wall (19th c);	45.055	43.859	42.705	1.196	2.35	25.88	650408	656379
Urban Block 12	4	To investigate for a possible town wall beside the River Breagagh	Reclamation deposits (18th-20th c) to natural subsoil; possible embankment of stones.	45.059	43.559	41.259	1.5	3.8	30.17	650427	656358
Urban Block 12	5	Investigate nature of reclaimed or made ground	Masonry wall; trench closed by the environmental ground sampling team because of ground pollution	45.249	44.149	44.149	1.1	1.1	27.33	650413	656414
Urban Block 12	6	Investigate the depth to top of archaeology in the Bull Inn	Fill deposits (later 19th c)	44.759	43.749	43.749	1.01	1.01	5.19	650370	656379



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Area	Trench No.	Trench Aims	Summary of Results	OD Top of Trench m	OD Top of Archaeology m	OD Base of Trench	Depth to Archaeology m	Depth of Trench m	Area of Trench m <sup>2</sup>	Centre ITM_E	Centre ITM_N
Urban Block 12	7	Investigate the depth to top of archaeology in the Bull Inn	Trench abandoned because of ground pollution: asbestos identified in made ground (late 20thc).	43.074		42.99	NA	0.084	4.52	650387	656362
Urban Block 13	1	To investigate possible water course and medieval/post-medieval made ground	Concrete wall foundations (20thc); peaty deposits to natural subsoils.	44.977	42.197	41.977	2.78	3	28.5	650440	656431
Urban Block 13	2	To investigate possible route of Mill Lane and medieval and post-medieval made ground	Modern surfaces; reclamation deposits (18th-20thc); peaty deposits to natural.	44.981	43.611	41.531	1.37	3.45	31.59	650462	656439
Urban Block 13	3	To investigate medieval and post-medieval made ground.	Modern surfaces; reclamation deposits (18th-20thc) to natural.	45.04	43.62	41.14	1.42	3.9	25.98	650469	656426
Urban Block 13	4	To investigate medieval and post-medieval made ground	Concrete walls and services (20th c.); reclamation deposits to natural subsoil.	45.029	43.319	42.99	1.71	2.039	25.87	650446	656399

## Proposed Development

The proposed development will provide parking spaces for seven coaches and one hundred and thirty-two car parking spaces. The proposed layout of the car and coach parking is as outlined below (see also Figure 12 and Figure 13). The car / coach park will be accessed from St Francis Bridge and the Central Access Scheme. A right turning lane will be provided from the Central Access Scheme into the car park.

The existing roadside boundary either side of the proposed entrance will be modified to provide sightlines of 45m in each direction. These sightlines will be in accordance with the requirements of the Design Manual for Urban Roads and Streets (DMURS), as published by the Department of Transport, Tourism and Sport and the Dept. of the Environment, Community and Local Government.

**Surface:** The proposed car and coach park will be constructed on top of the existing concrete slab, with the slab to be overlaid with a bituminous layer to provide a level surface (Figure 13: area c. 4,750m<sup>2</sup>).

The north west corner of the site, which is currently lower than the surrounding concrete slab will be raised by approximately 500mm to match the level of the concrete slab (Figure 13: area c. 601m<sup>2</sup>). The raised area will be stopped approximately 3m away from the upstanding wall of the 'Bull Inn'.

The level of a localised raised area will be reduced by the removal of the existing concrete slab (c. 0.3m) to provide a consistent level to the site (Figure 13: area c. 320m<sup>2</sup>).

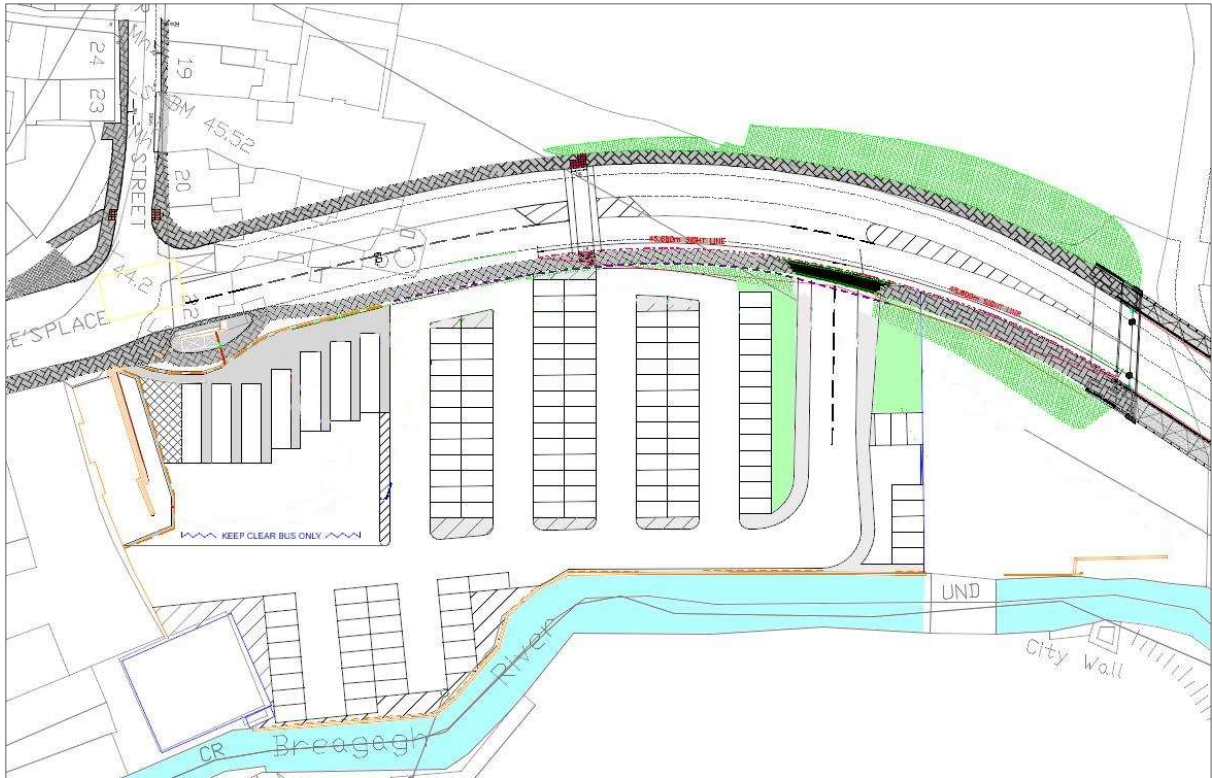


Figure 12: Layout of the proposed car / coach park

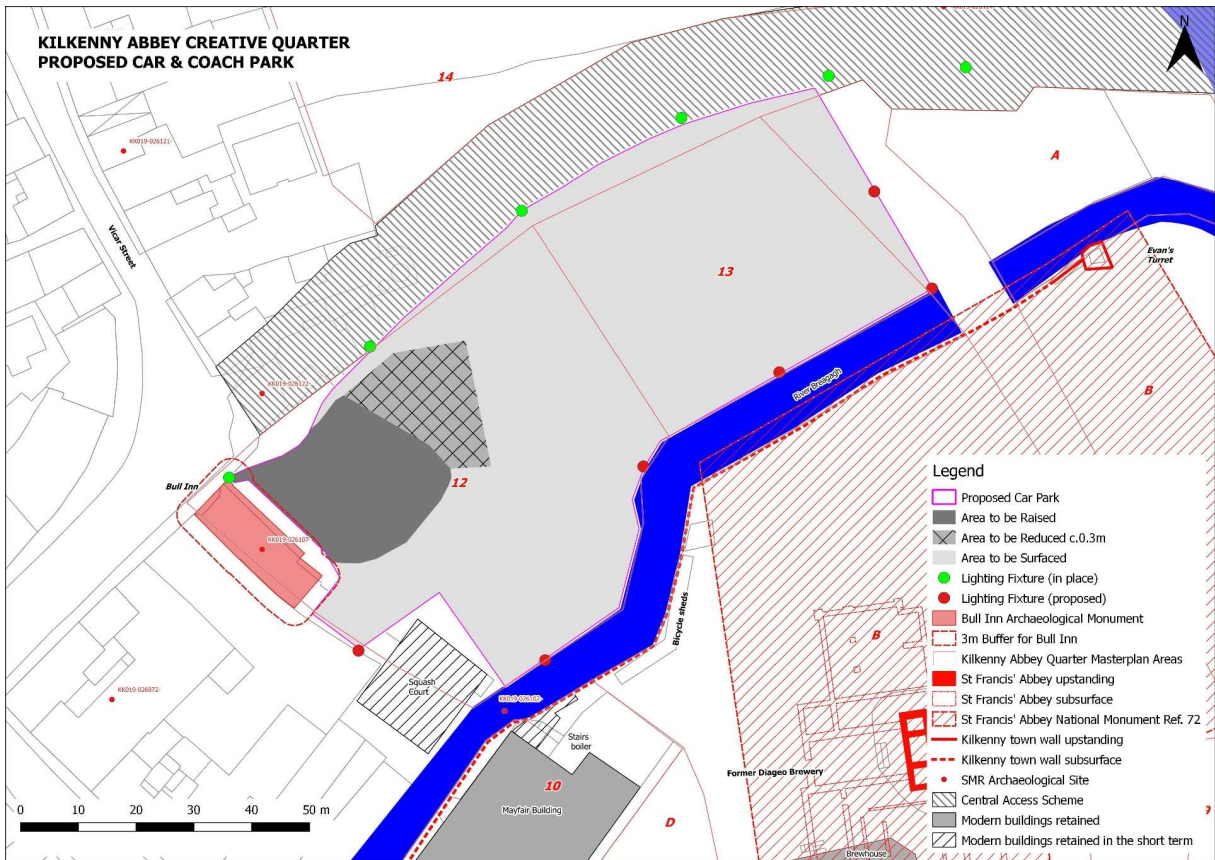


Figure 13: Proposed development and recorded archaeology

**Drainage:** The proposed parking area will use the existing drainage system in the concrete yard. There are currently two surface water outfalls to the River Breagagh from this concrete yard, both of which incorporate petrol interceptors. These outfalls will be used to discharge surface water run-off from the car / coach park to the River Breagagh. It is not intended to construct any new outfall as part of the proposed development.

**Southern Boundary:** The existing boundary wall along the River Breagagh is lower than the required 1.2m high boundary along the river. Accordingly, it is proposed to provide a 1.2m high timber fence along this boundary with the fence posts to be bolted to the concrete slab to avoid unnecessary excavations in this area. This timber fence will be similar to the existing timber fencing that has been used on the Central Access Scheme leading to St Francis Bridge.

**Western Boundary:** In order to provide protection to the walls of the 'Bull Inn' (on the western boundary of the car park) it is proposed to provide a buffer zone, up to 3m wide. This boundary will be formed using 2.0m high timber fencing. In order to avoid excavation in this area, it is proposed that the supports for the required fencing will be provided by way of concrete blocks that will be placed on top of the existing surface. These concrete blocks will also retain the material that is to be used to raise the existing ground levels in this area. An information panel will be provided on this fencing that will provide visitors to the parking with details of the history of the 'Bull Inn'.

**Eastern Boundary:** A green powder coated palisade or paladin type fence will be provided on the eastern boundary of the site, immediately adjoining the existing bridge crossing of the River Breagagh. This will allow for views from the site to the River and to Kilkenny City walls including Evans Turret. This fence will be bolted to the concrete slab. A gate will be incorporated into the fencing to allow for occasional access for construction traffic to the balance of the Abbey Quarter site.

**Northern Boundary:** The existing timber fence will be retained from St Francis Bridge to the entrance to the proposed car park. A section of the existing hedging in this area will need to be removed to provide for the required sightlines from the proposed entrance. To the west of the proposed entrance the existing timber fence will be removed and modified as necessary to open the car park site up to pedestrian traffic. The existing palisade fence will be removed from this boundary.

**Public Lighting:** Public lighting will be provided in the car park to provide a satisfactory level of lighting at night time. The existing public lighting heads on the Central Access Scheme leading to St Francis Bridge will be modified by way of installing a second lighting head that will be directed into the car / coach park site. Additional lighting will be provided along the western, eastern and southern boundaries of the site (Figure 13). The area of the required concrete bases for six poles along the west (1), east (1) and south (4) boundaries will be excavated and monitored by an archaeologist. The

cabling between the lighting poles will be located above ground and will be fixed to the fencing along these boundaries. Light spillage onto the adjoining River Breagagh will be limited to less than 1.0 Lux to avoid impact on the River Breagagh.

## Archaeological Impact of the Proposed Development

The proposed development has been designed to avoid impacting on the upstanding and subsurface archaeology in the development area. These archaeological features include:

- The Bull Inn (SMR KK019-026107-) comprising upstanding remains of a masonry structure from the early seventeenth century.
- Subsurface remains of buildings and occupation levels identified during test excavations (17E0642) in Trenches 1 and 2, Urban Block 12, at least 0.5m below the current ground level. These would be considered part of the historic town of Kilkenny (RMP KK019-026). The preliminary interpretation of these buildings is that they are the remains of early seventeenth to eighteenth century buildings possibly associated with the 'Bull Inn', depicted as upstanding into the middle of the nineteenth century on historical maps.
- Upstanding remains of Evans Turret and Kilkenny town walls, part of a National Monument (DoEHLG 2008), located on the opposite bank of the River Breaghagh and close to, but outside the area of the proposed development.

St Francis' Abbey (SMR KK019-026101-), a National Monument (No.72), is located outside and c. 70m south of proposed development and will therefore not be impacted by the proposed development.

A gable structure for No. 22 Vicar Street preserved in-situ during the development of the Central Access Scheme Project is also located outside the proposed development (Plate 4). This structure is secured with bracing and surrounded by a timber fence and will not be impacted by the proposed development.

The proposed reduction of ground (Figure 13: area 320m<sup>2</sup>) will only be to a depth of the existing concrete slab (c. 0.30m). Archaeology in this area of the site is thought to be at least 1m below modern ground surface levels, based on the findings of surrounding excavations and from geotechnical investigations in TP01.

## Mitigation Measures

Kilkenny County Council have designed the proposed development to minimise the impact on the archaeology. These measures are:

- To protect the Bull Inn monument it is proposed to provide a buffer zone, up to 3 m wide, with a boundary between the car park and the Bull Inn. This boundary will be formed using 2.0m high timber fencing. This fence will protect the Bull Inn monument during the construction, operation and decommissioning of the proposed car park. In order to avoid excavation in this area, the supports for the required fencing will be provided by way of concrete blocks that will be placed on top of the existing surface. These concrete blocks will also retain the material that is to be used to raise the existing ground levels in this area. An information panel will be provided on this fencing that will provide visitors to the parking with details of the history of the Bull Inn.
- The raised area will be kept at least 3m away from the upstanding wall of the Bull Inn archaeological monument (KK019-026107-).
- The proposed parking area will use the existing drainage system in the concrete yard, avoiding the need to excavate new drainage services.
- A palisade-type fence on the eastern boundary of the site, immediately adjoining the existing bridge crossing of the River Breagagh will allow views from the site to the River and to the City Wall including Evans Turret, to mitigate the impact on visual amenities of these features, and protecting these features from impacts during the construction, operation and eventual decommissioning of the car park.
- Cabling between the lighting poles surrounding the site will be located above-ground and will be fixed to the fencing along these boundaries. Where possible (along the northern boundary of the proposed car park) existing lighting poles will be used to carry lighting for the car park. The car park lighting was also designed to avoid impacting the areas of identified archaeology such as the Bull Inn.
- The excavation of required concrete bases for six new lighting poles will be monitored by an archaeologist (Figure 13).

In addition to the above, a licence eligible archaeologist should be employed to monitor ground works during construction, including:

- The placement of the protective concrete blocks and fencing around the Bull Inn
- The proposed reduction of ground levels in the car and coach park area

This monitoring should be carried out under licence from the National Monuments Service. If archaeological features are impacted or newly discovered during the development construction works then works in that area should stop, the archaeology fenced off and protected, the National Monuments Service and Kilkenny County Council informed, and a methodology agreed to mitigate these impacts.

These recommendations are subject to the approval of the National Monuments Service.

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